



Homes
England

The Housing and Regeneration Agency

West of Ifield, Crawley Design Code

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Version 1 - Planning submission

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INTRODUCTION



1.1 PURPOSE OF THE DOCUMENT

The Design Code has been prepared on behalf of Homes England (the 'Applicant') as part of an Hybrid Planning Application (HPA).

The HPA for West of Ifield is supported by Parameter Plans for approval that will establish parameters for the development in the following areas: Landscape and Public Realm, Movement and Access, Land Use and Building Heights.

A Development Specification and Parameter Plan Framework is also submitted for approval which defines the type and quantum of development that can be brought forward in different areas of the site at Reserved Matters Application stage (RMA).

The aim of the Site-Wide Design Code is to provide clarity over what constitutes acceptable design quality for a particular site or area, and thereby provide a level of certainty for the Local Planning Authority (LPA), developers and the local stakeholders. The Site-Wide Design Code provides mostly HPA guidance to inform detailed design proposals and builds on the design principles set out in the Design and Access Statement (DAS).

The Design Code has been prepared with the full knowledge that it may be varied and/or amended over time with the agreement of the LPA. It needs to be capable of evolving and responding to changes which might arise throughout the build programme and needs to maintain a degree of flexibility in implementation.

Who will the Design Code be used by?

The Design Code will be a document used by applicants in preparing RMAs as well as the Local Authority in assessing those applications and determining whether compliance, and required standards of design have been met.

Local community members and stakeholders will also be able to read the Design Code to understand the expectations for design quality and have certainty about what will be built.

The Town and Country Planning (Development Management Procedure, England) Order 2015 (DMPO) sets out the requirements for outline planning applications and allows for specific details of the application to be reserved for subsequent approval by the LPA under subsequent RMAs.

HYBRID PLANNING APPLICATION

This HPA is submitted with a series of documents that have been agreed in discussion with the local planning authority and in line with the local validation requirements.

HYBRID

Application Forms and Certificates

Provide statutory planning application forms being submitted to the local planning authority.
This includes the Application Form and Certificates and Community Infrastructure Levy Form.

Approval drawings

Provide detailed drawings for assessment of the hybrid element of the HPA.
This includes Planning Application Boundary (Red Line) Plan, Site Location Plan, Building Demolition / Retention Plan, Landscape Retention Plan, and Public Right of Way Amendment Plan.

Technical Assessments and Strategies (applicable to both Detailed & Outline elements)

Provide details in regard to technical assessment and strategies that have informed the impact assessments, proposed mitigation and infrastructure requirements across the Detailed & Outline elements.

This includes the documents set out within the 'supporting technical documents' list below.

DETAILED

Approval drawings

Provide detailed drawings for assessment of the detailed element of the HPA.
This includes site plans, section drawings, highways drawings, drainage drawings and landscaping plans.

Technical Assessments and Strategies (applicable to Detailed element only)

Provide details in regard to technical assessment and strategies that have informed the impact assessments, proposed mitigation and infrastructure requirements across the Detailed elements of the HPA.

This includes the detailed highways drawings (general arrangement, paving layouts, long sections and cross sections), detailed drainage drawings and detailed landscaping and planting drawings.

OUTLINE

Development Parameters

Provide the description of development and parameters for which outline approval is sought. Future RMAs would be required to demonstrate compliance with these parameters.
This includes the Development Specification Framework, Site Wide Design Code, and Parameter Plans.

Information & Illustration

Provide plans, frameworks and illustrative material that demonstrates how development could be brought forward.
This includes the Illustrative Masterplan and Existing Levels Plan.

SUBMISSION, DETERMINATION AND APPROVAL

The HPA submission documents will be reviewed and determined by the local planning authority. A Decision Notice will be accompanied by a Section 106 Agreement and Schedule of Conditions.

Section 106 Agreement

The consent is accompanied by a Section 106 Agreement that binds legal obligations and financial commitments.

Decision Notice

The Decision Notice provides written assessment and justified reasoning for the approval of the HPA.

Schedule of Conditions

The consent is accompanied by a series of conditions.

CONDITIONS

Conditions will be attached to both the detailed and outline elements of the permission, and will likely include pre-commencement, pre-occupation and compliance conditions for each phase of the permission. Pre-commencement conditions will be agreed with the local planning authority prior to the determination of the HPA.

Site-Wide, Pre-Commencement and Compliance Conditions

For example Open Space Strategy, Public Art Strategy, Lighting Strategy, and others as agreed.

Full (Detailed) Only Conditions

This is likely to include pre-commencement conditions for Phase 1.

Outline Only Conditions

This is likely to include pre-commencement conditions for Character Design Codes (relating to RMAs), and others as agreed.

CONSTRUCTION

Construction of Phase 1.

Construction of Phase 2 to Phase 5.

RMAs

RMAs will be brought forward in compliance with the Development Parameters, any subsequently approved conditioned details and in accordance with obligations of the S106 Agreement and the Design Codes.

1.2 DOCUMENT STRUCTURE

Addressing each of the characteristics in the National Model Design Code, the West of Ifield Design Code includes Site-Wide Design Coding and Outline Character Area Coding.

Site-Wide Design Codes

The Site-Wide Design Codes sets out the minimum design expectations future applicants must achieve. Site Wide Codes are provided for:

1. Movement and Access
2. Land Use
3. Landscape and Open Space
4. Built Form
5. Sustainability

Outline Character Area Design Codes

The Design Code also includes initial guidance specific to each character area in the Masterplan. This will ensure the development in different areas remains distinct and responsive to the range of different site conditions including context and setting. It is envisaged that Detailed Character Area Design Codes will be approved ahead of commencement of development of the relevant Character Area.

The structure of the Design Coding follows a consistent format as shown in the typical page layout shown in Figure 1. The contents of these pages are intended to be used as follows:

Headings and Sub-headings

Headings and sub-headings in each section are numbered for reference and clearly state the topic addressed.

Vision and Objective

A short statement which sets out the Vision for this chapter and sets out the design intent for each element.

HPA Coding

These codes are provided to ensure future applicants comply with the parameters and design requirements of the hybrid planning application.

Reserved for Future Design Stages

These statements are provided to support future applicants and the LPA in considering matters which will be fundamental to securing high quality design but are not prescribed within the HPA. This is to recognise a level of design and resolution which it is not appropriate to fix under the HPA.

Precedents and Examples

In some cases the document highlights examples which are considered to represent strong built examples of elements that would be consistent with the code. These examples are accompanied by descriptions where appropriate to explain why they are good examples to be followed, or bad examples to be avoided.

Illustrative Diagrams

Illustrations and diagrams are provided throughout the document to show how the development could be designed in a manner that is considered to be consistent with the code.

Headings and Sub-headings

Vision and Objectives

HPA Coding

Reserved for Future Design Stages

• 3.1 MOVEMENT AND ACCESS

Vision and Objectives: West of Ifield will be a place where walking, cycling and active transport are prioritised, distinct from the private car for local journeys. Every day services, shops, employment uses and leisure facilities will be easily accessible by these modes in a safe and sustainable way.

HPA Coding

1. Maximising Active Travel: Design of the transport street network must reflect the modal hierarchy, which is based on maximising active travel and minimising the need for day-to-day car use. The hierarchy is as follows (most important first):

- Walking;
- Cycling;
- Public Transport Service (Local Bus);
- Shared Vehicles and Taxis;
- Service and logistics vehicles; and
- Private cars

2. Connecting off-site Routes: New streets and pedestrian/cycle routes must directly connect with off-site routes, including Public Rights of Way (PROW) shown in Figure 100.

3. Hierarchy of Street Types: A clear hierarchy of primary, secondary and tertiary streets must be established that provides for mobility options appropriate to the location/ neighbourhood.

4. People Focused: Streets must be designed as shared spaces accommodating all individuals, not exclusively for vehicular use. Designing them to support reduced vehicle speeds (typical around 20mph), prioritises a human-focused environment in adherence to street design standards.

Reserved for Future Design Stages

• Streets as both movement corridor and well-designed places: Future detailed codes should demonstrate how measures have been taken to prevent streets becoming traffic-dominated and therefore allowing the design of high quality public realm.

• Vehicle Networks and Car Parking Locations: Future detailed codes should demonstrate how designs allow for future flexibility and to ensure adaptability for future changes in lifestyles, movement patterns, technology and demand.

FIGURE 6 Maximising opportunities for Active Travel - Cycling [1]

FIGURE 8 A hierarchy of Street Types [3]

FIGURE 7 Connecting off-site Routes [2]

FIGURE 9 Strategically placed greenery/trees serve to constrict the street, encouraging drivers to reduce their speed. [4]

KEY

Context

- Hybrid Application Area
- Existing Vehicular Links
- Green Infrastructure
- Water Course
- Attenuation Ponds
- Building Zone
- Existing PROW (outside of red line boundary)

Details defined in Detailed Proposals

- Area Applied in Detail
- Areas included within the detailed application but where RMAS will be submitted in the future.
- Landscape delivered under the detailed element

• CWMCC

• The Primary Street

• Bus Gate with Pedestrian and Cycle Access

• Bus stop

• Access Point - All Modes

• Crossing Point - All Modes

• Crossing Point - Pedestrian and Cycle Only

Details defined in Parameter Plan (fixed components)

- Primary Roads
- Pedestrian and Cycle Links
- Existing PROW within Site (enhanced and upgraded)

• Access Point - Pedestrian and Cycle Only

• Access Point - Pedestrian Only

Details indicated in Illustrative Masterplan (flexible components)

- Indicative Secondary Roads

FIGURE 10 Movement and Access Plan

Precedents and Examples

Illustrative Diagram

FIGURE 1 Typical page within the Design Code



2

WEST OF IFIELD



2.1 THE VISION

West of Ifield will be a well-designed place with a thriving community. The local centre with community uses, retail, employment, primary school and secondary school will form the heart of the neighbourhood, creating a place to gather and socialise. A good mix of formal and informal green spaces will offer a variety of recreational and leisure opportunities. Residents will benefit from a broad range of new homes adapted to different stages of life, including a variety of tenure choices.

West of Ifield will deliver sustainable homes and work places within an expansive network of green spaces. Landscaped spaces will be drawn into the heart of the neighbourhood creating a focus for views and activity and reinforcing connections with nature.

The design of streets and buildings will draw upon local character to ensure that the neighbourhood feels part of the local area, but at the same time, create a place which is distinctive in its own right.

West of Ifield will be connected by a range of transport choices and opportunities. Proposals will maximise opportunities for the use of sustainable travel modes, particularly walking, cycling and public transport. Neighbourhoods in West of Ifield will include safe and attractive pedestrian and cycle routes, high-quality public transport serving local employment areas, places of education and the wider area. It will be a place that considers the mobility needs of the whole community through an inclusive design approach.

The Illustrative Masterplan illustrates one way in which the Parameter Plans can be interpreted to inform a masterplan. The Illustrative Masterplan also acts as a proving plan that has tested the principles and guidance established in the Site Wide Design Code.

On the opposite page the 10 key vision principles are expanded and explained and they underpin all the guidance in these Design Codes.

Key for Masterplan

- ① Crawley Western Multi-Modal Corridor
- ② River Valley Country Park
- ③ Meadow Park
- ④ The Grove Sports Hub
- ⑤ Local Centre
- ⑥ Primary School
- ⑦ Secondary School
- ⑧ Ridgeway Park



FIGURE 2 Illustrative Masterplan

West of Ifield will...



NATURE AND LANDSCAPE

- + Support active healthy lifestyles.
- + Accommodate 50% open space.
- + Support mitigation and adaptation of climate change.
- + Be sensitive to the surrounding rural landscape and communities.
- + Support diverse activities across the development.
- + Connect residents with nature through recreation, play & education.



ENVIRONMENT

- + Celebrate, protect, and enhance the value of key ecological and landscape features at the site.
- + Maximise positive and minimise any adverse impacts on the environment.
- + Provide at least 10% net biodiversity net gain.



MOVEMENT

- + Prioritise walking, cycling and public transport over car travel.
- + Have streets that allow for a variety of uses, including play.
- + Accommodate new and active forms of transport (e.g. electric scooters and cycles) and anticipate the onset of 'mobility as a service'.
- + Minimise car parking, with the ability to be reduced further over time.
- + Provide routes that allow for easy access to other employment centres and the wider area.



SUPPORTING INFRASTRUCTURE

- + Have the potential to provide on-site energy generation.
- + Design out unnecessary servicing vehicular movements.
- + Provide high quality digital connectivity for businesses.



STEWARDSHIP

- + Consider long-term stewardship and community involvement at every stage.



LIVING

- + Provide flexible, well-designed homes.
- + Accommodate a range and mix of tenures and typologies.
- + Offer accessible and attractive homes for people who currently live locally as well as further afield.



WORKING

- + Provide a range of workspaces.
- + Allow for flexible, modern working patterns which support good work-life balance.
- + Promote an economy that complements and supports the growth of Manor Royal, Gatwick, North Horsham and the wider economic area, supported by access to mainline rail services.



INTERACTION

- + Provide opportunities for community interaction, in order to combat social isolation.
- + Accommodate meanwhile uses during development
- + Encourage residents to influence how community spaces develop.
- + Have a strong cultural offering to serve both the community and wider area.
- + Be an inclusive, accessible place that connects with existing communities.



LEARNING

- + Encourage learning across lifetimes.
- + Establish learning within the community, using schools and other community centres as a place for interaction.



HEALTH

- + Be designed to encourage active and healthy lifestyles.
- + Offer a range of recreational and community facilities to ensure residents are encouraged to stay healthy.

2.2 SITE OVERVIEW

The Site extends to 171.29 hectares and predominantly consists of flat agricultural land.

The development lies within Horsham District, except for a footpath link across Ifield Meadows and access on Ifield Avenue, which both are located within Crawley Borough.

There are areas of nearby green space, such as Ifield Meadows, which are also owned by Homes England. These areas are not within the HPA, but form part of the Site's context and are valuable assets to the local community.

The Site borders the western edge of the existing neighbourhood of Ifield. The residential neighbourhoods of Ifield West, Bewbush, Glossops Green and Langley Green also lie in close proximity. The southern part of the Site is characterised by Ifield Golf Club.

The western part of the Site is largely undulating agricultural land characterised by areas of ancient woodland and field boundaries. The River Mole bisects the Site to the north with Ifield Wood Road immediately north of this.

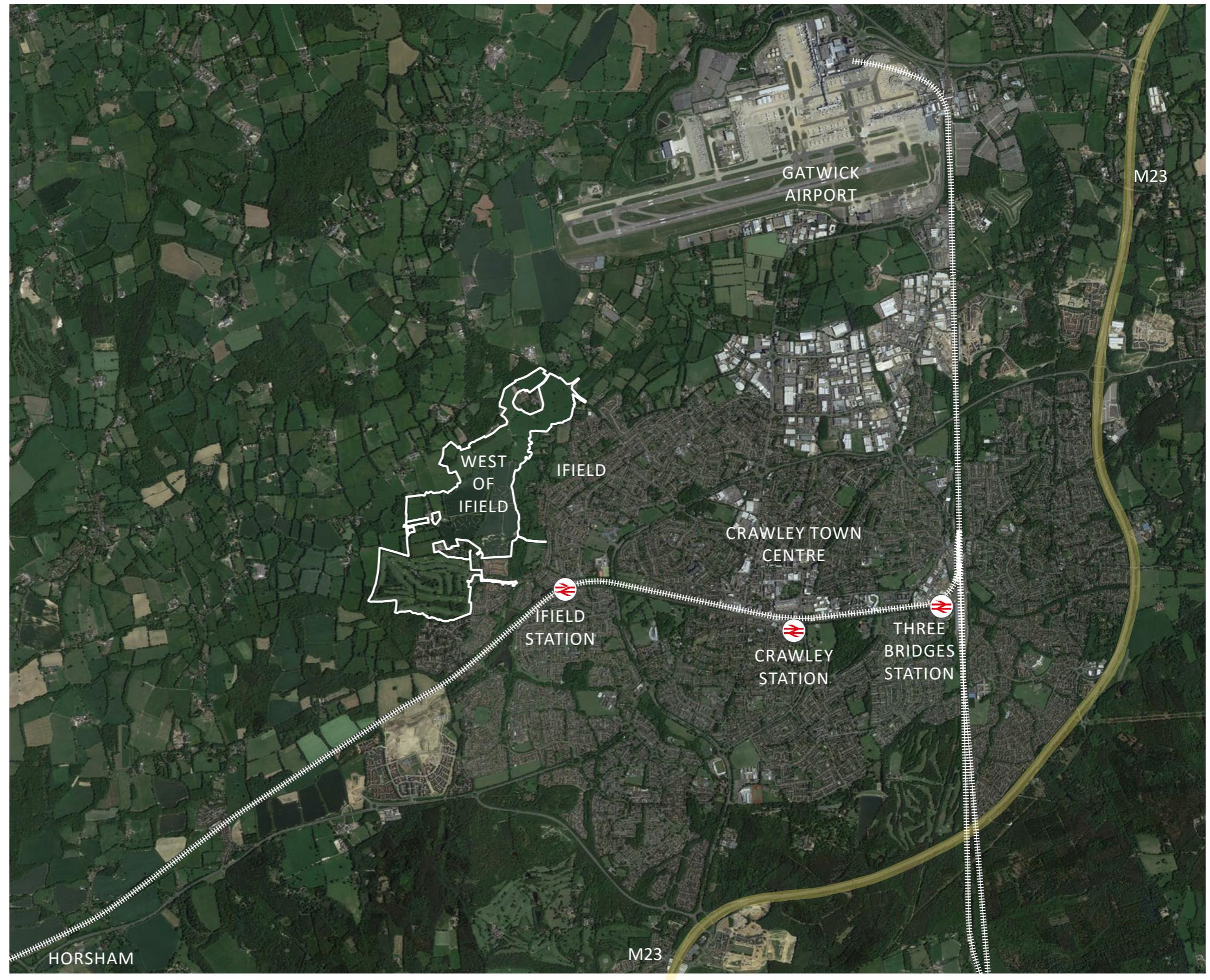


FIGURE 3 Land West of Ifield - wider context

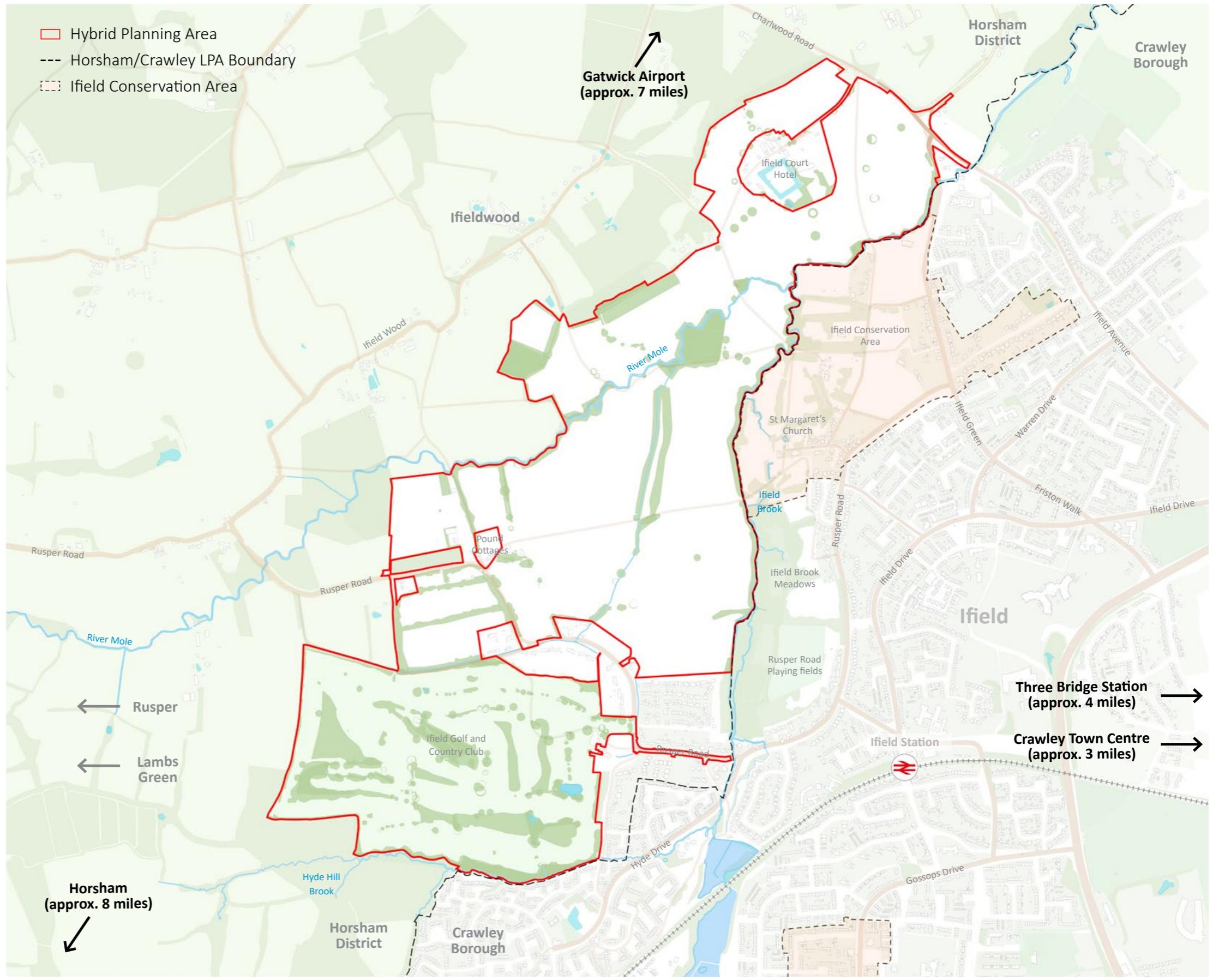


FIGURE 4 Hybrid Planning Area

2.3 AREA APPLIED FOR IN DETAIL

2.3.1 DETAILED PROPOSALS

A primary focus of the detailed proposal is the implementation of Phase 1 infrastructure necessary to deliver the Secondary School, which requires careful coordination to ensure seamless integration with future development phases.

A critical component of the detailed proposal is the integration of transport and movement infrastructure. The Crawley Western Multi-Modal Corridor (CWMMC) and the Primary Street are designed to facilitate both immediate and long-term accessibility. The detailed proposal provides precise specifications for these corridors, including lane configurations, active travel provisions, and junction treatments, ensuring their functionality within the broader masterplan.

Building upon the outline proposal, the detailed proposal ensures that infrastructure is delivered efficiently, supporting the wider landscape strategy, transport strategy, and designated land uses as outlined in the strategic framework. At the same time, it allows for future modifications and enhancements as subsequent phases progress, ensuring adaptability to evolving development needs.

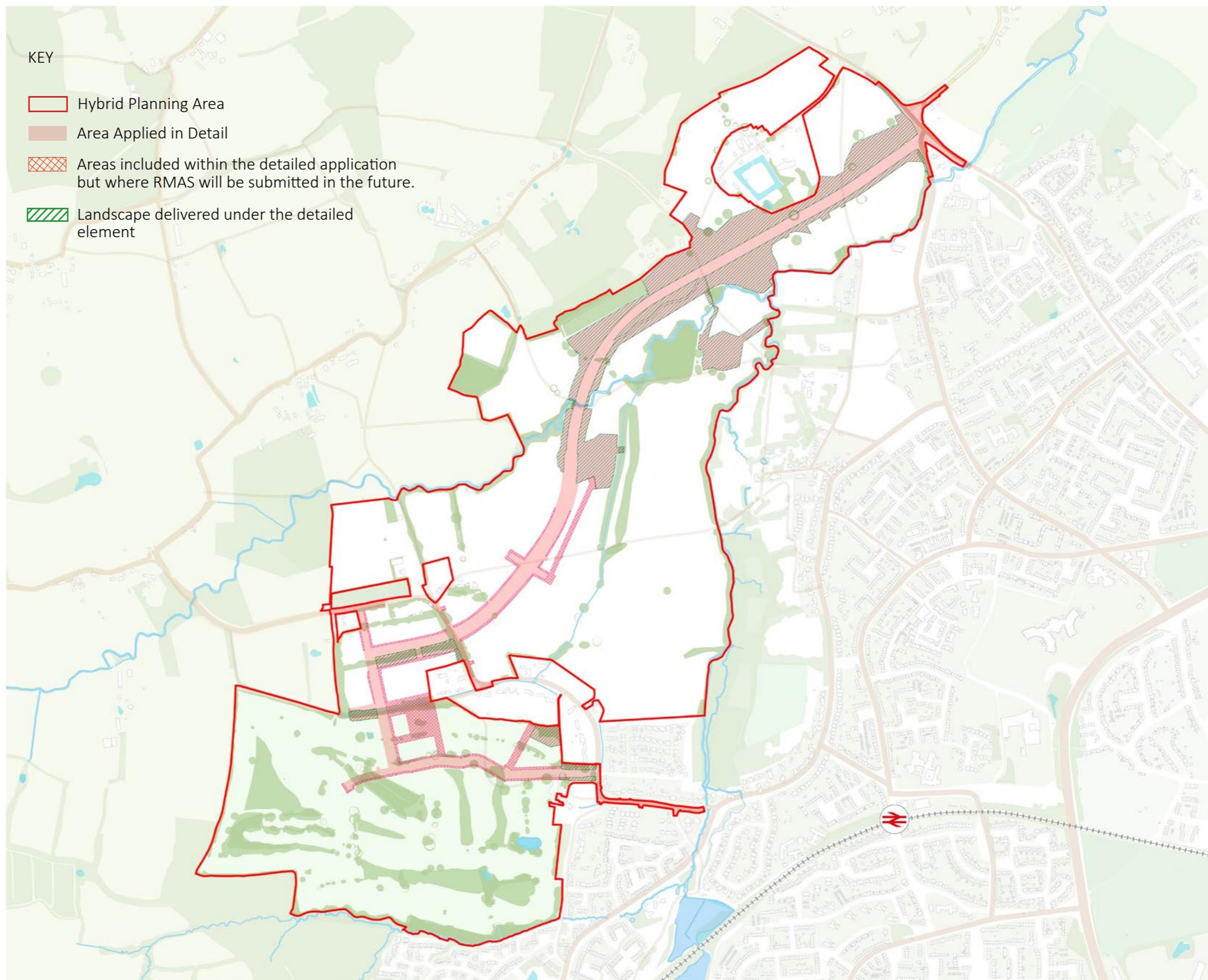


FIGURE 5 Planning Application Boundary

2.3.2 RELATIONSHIP WITH OUTLINE PROPOSALS

The hatched areas included within the detailed application represent locations where the detailed proposal overlays the outline proposal.

These areas will be further defined through RMAs in the future, detailing how boundaries and frontages will be integrated to ensure continuity and coherence in the final built environment. Further details are set out in the DAS.

Key considerations in such areas will be:



Reserved for Future Design Stages

- **Plot-Edge & Frontage Design:** Future detailed Character Area codes should define principles for how buildings interact with streets, ensuring active frontages, appropriate setbacks, and clear public-private boundaries.
- **Building & Infrastructure Interface:** Future detailed Character Area codes should provide guidance on transitions between plots and roads, including access points, pedestrian permeability, and landscape buffers.
- **Materiality & Public Realm Integration:** Future detailed Character Area codes should establish consistency in paving, boundary treatments, and street furniture to create a coherent sense of place.
- **Servicing & Access:** Future detailed Character Area codes should define outline parameters for parking, servicing arrangements, and shared surface strategies to avoid conflicts between different users.



SITE-WIDE DESIGN CODE



3.1 MOVEMENT AND ACCESS

Vision and Objectives: West of Ifield will be a place where walking, cycling and public transport are prioritised, discouraging use of the private car for local journeys. Every day services, shops, employment uses and leisure facilities will be easily accessible by these modes in a safe and sustainable way.



OPA Coding

1. Maximising Active Travel: Design of the transport and street networks must reflect the modal hierarchy, which is based on maximising active travel and minimising the need for day-to-day car use. The hierarchy is as follows (most important first):

- Walking;
- Cycling;
- Public Transport Service (Local Bus);
- Shared Vehicles and Taxis;
- Service and logistics vehicles; and
- Private cars

2. Connecting off-site Routes: New streets and pedestrian/cycle routes must directly connect with off-site routes, including Public Rights of Way (PRoW), as shown opposite in Figure 10.

3. Hierarchy of Street Types: A clear hierarchy of primary, secondary and tertiary streets must be established that provide for mobility options appropriate to the location/ neighbourhood.

4. People Focussed: Streets must be designed as shared spaces accommodating all individuals, not exclusively for vehicular use. Designing them to support reduced vehicle speeds (typically around 20mph), prioritises a human-focused environment in adherence to street design standards.



Reserved for Future Design Stages

- **Streets as both movement corridors and well-designed places:** Future detailed codes should demonstrate how measures have been taken to prevent streets becoming traffic-dominated and therefore allowing the design of high quality public realm.
- **Vehicle Networks and Car Parking Locations:** Future detailed codes should demonstrate how designs allow for future flexibility and to ensure adaptability for future changes in lifestyles, movement patterns, technology and demand.



FIGURE 6 Maximising opportunities for Active Travel - Cycling [1]



FIGURE 8 A hierarchy of Street Types [3]



FIGURE 7 Connecting off-site Routes [2]



FIGURE 9 Strategically placed greenery/trees serve to constrict the street, encouraging drivers to reduce their speed. [4]

KEY

Context

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Details defined in Detailed Proposals

- Area Applied in Detail
- Areas included within the detailed application but where RMAS will be submitted in the future.
- Landscape delivered under the detailed element
- CWMMC
- The Primary Street
- Bus Gate with Pedestrian and Cycle Access
- Bus stop
- Access Point- All Modes
- Crossing Point- All Modes
- Crossing Point- Pedestrian and Cycle Only

Details defined in Parameter Plan (fixed components)

- Primary Roads
- Pedestrian and Cycle Links
- Existing PRoW within Site (enhanced and upgraded)
- Access Point- Pedestrian and Cycle Only
- Access Point- Pedestrian Only

Details indicated in Illustrative Masterplan (flexible components)

- Indicative Secondary Roads

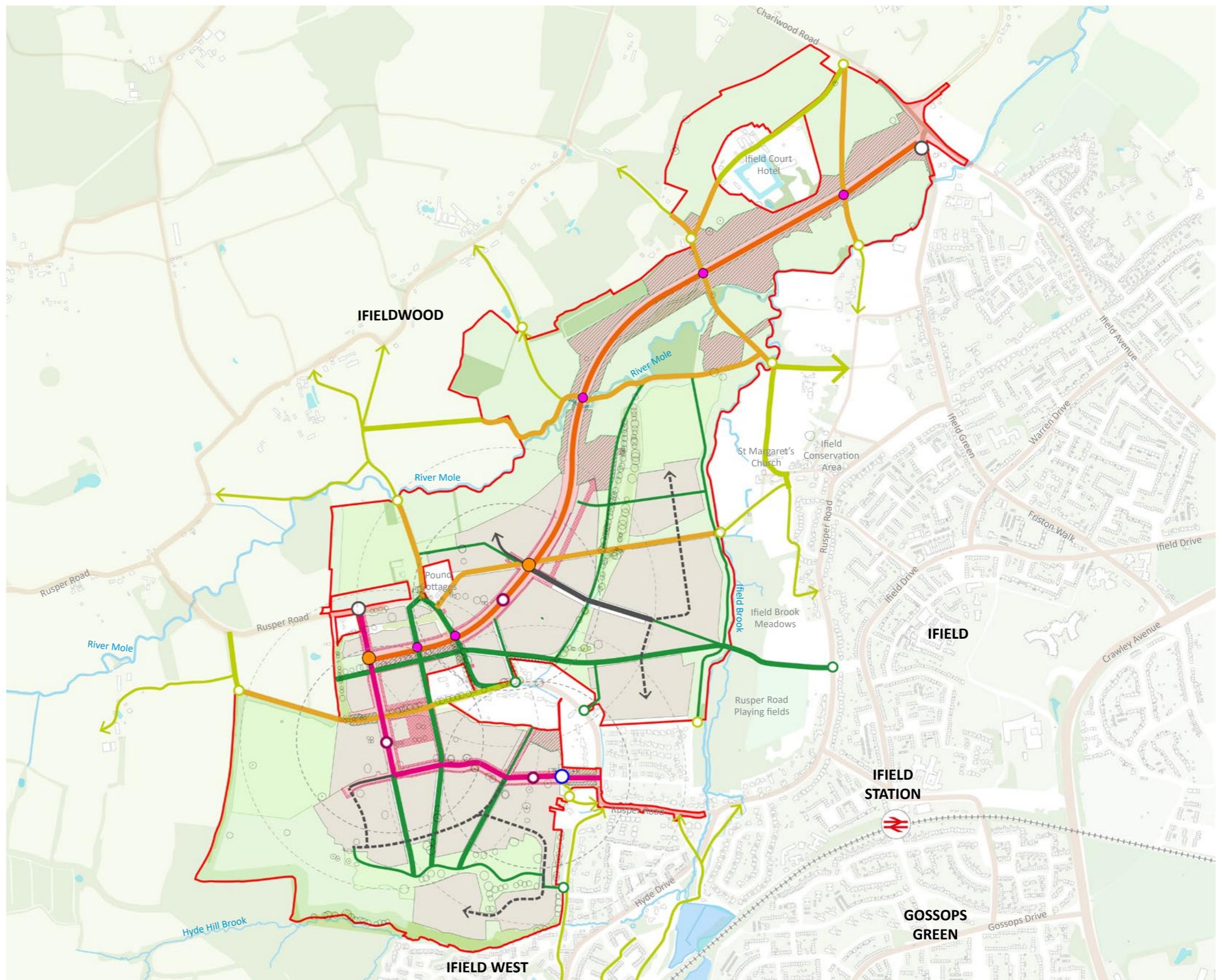


FIGURE 10 Movement and Access Plan

3.1.1 PEDESTRIAN, CYCLE AND MICRO-MOBILITY

Vision and Objectives: West of Ifield will be a place where active and sustainable travel is the natural choice, establishing walking, cycling and micro-mobility routes which aim to be more direct, quicker and cheaper for local trips than using a car.

The strategy will define how a network of segregated on-street facilities and traffic-free routes provide connectivity both to on-site and off-site routes, including Public Rights of Way (PRoW).



OPA Coding

1. Traffic-free Routes: A network of traffic-free legible routes must be established as shown in Figure 15, making use of green corridors and existing PRoW. All residents must be within easy access to a traffic-free route.

2. Cycle and Micro-mobility Routes: Dedicated routes (physically separated from motor vehicle or pedestrians) must be provided on the primary street. Cycle routes must be a minimum of 1.5m (one-way width), or latest approved design standards.

3. Existing PRoW: Existing PRoW within the development boundary must be improved and seamlessly integrated into the overall design. In cases where the existing PRoW is deemed inappropriate, an alternative route must be provided to enhance the overall connection.

4. Linking Neighbourhoods to Parks: Neighbourhood pedestrian and cycle routes must be easily connected to the strategic mobility corridors within Ridgeway Park, Meadows Park and River Valley Park, enhancing accessibility for walking and cycling trips.

5. Connecting off-site Routes: Existing PRoW must be connected to routes within the development and therefore ensure access is provided to the surrounding countryside and nearby settlements.

6. Linking Routes to Public Spaces: Pedestrian and cycle routes must link to new public spaces within the development to create opportunities for social gathering, with natural surveillance along the route.

7. Crossings: Pedestrian and cycle crossings must be provided as defined within the parameter plans.

Reserved for Future Design Stages

- Effectively Integrating Cyclists with Pedestrians:** Future detailed codes should demonstrate how the interaction of pedestrians from cyclists on shared routes has been considered, reflecting latest appropriate guidance and design standards at time of submission.

- Leisure Routes:** Future detailed codes should introduce a wellness and fitness trail that seamlessly connects pedestrian and cycle routes, fostering uninterrupted movement between character areas. The design aims to minimise disruptions for cyclists, reducing the need to frequently stop at lights or yield to traffic. Please refer to Figure 16 for a more detailed indicative leisure routes plan.

- Way-finding and Signage:** Future detailed codes should demonstrate how way-finding and signage has been integrated to be inclusive and consistent. These should also explain the key landscapes, heritage and natural features.

- Routes Design:** Future detailed codes should demonstrate a route design harmonious with the context, especially within the Local Green Space designation, utilising natural materials and an organic layout, coupled with thoughtful lighting. The aim is to preserve the natural essence of the space, avoiding excessive urbanisation or the creation of wide strip of tarmac that disrupt the areas integrity.



FIGURE 11 Traffic-free Route within open space [1]



FIGURE 13 Dedicated cycle and micro-mobility routes [2]



FIGURE 12 Traffic-free Route within neighbourhood [2]

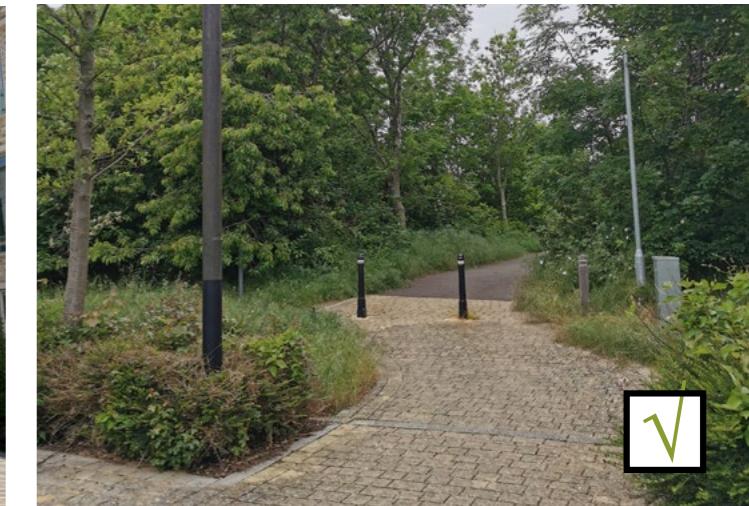


FIGURE 14 Connecting off-site routes [5]

KEY

Context

- Hybrid Planning Area
- Existing Vehicular Links
- Green Infrastructure
- Water Course
- Attenuation Ponds
- Building Zone
- Existing PRoW

Details defined in Detailed Proposes

- Area Applied in Detail
- Areas included within the detailed application but where RMAS will be submitted in the future.
- Landscape delivered under the detailed element
- CWMMC
- The Primary Street
- Bus Gate with Pedestrian and Cycle Access
- Bus stop
- Access Point- All Modes
- Crossing Point- All Modes
- Crossing Point- Pedestrian and Cycle Only

Details defined in Parameter Plan (fixed components)

- Streets with Segregated Cycle Lanes
- Pedestrian and Cycle Links
- Existing PRoW within Site (enhanced and upgraded)
- Access Point- Pedestrian and Cycle Only
- Access Point- Pedestrian Only

Details indicated in Illustrative Masterplan (flexible components)

- Indicative on-street connections

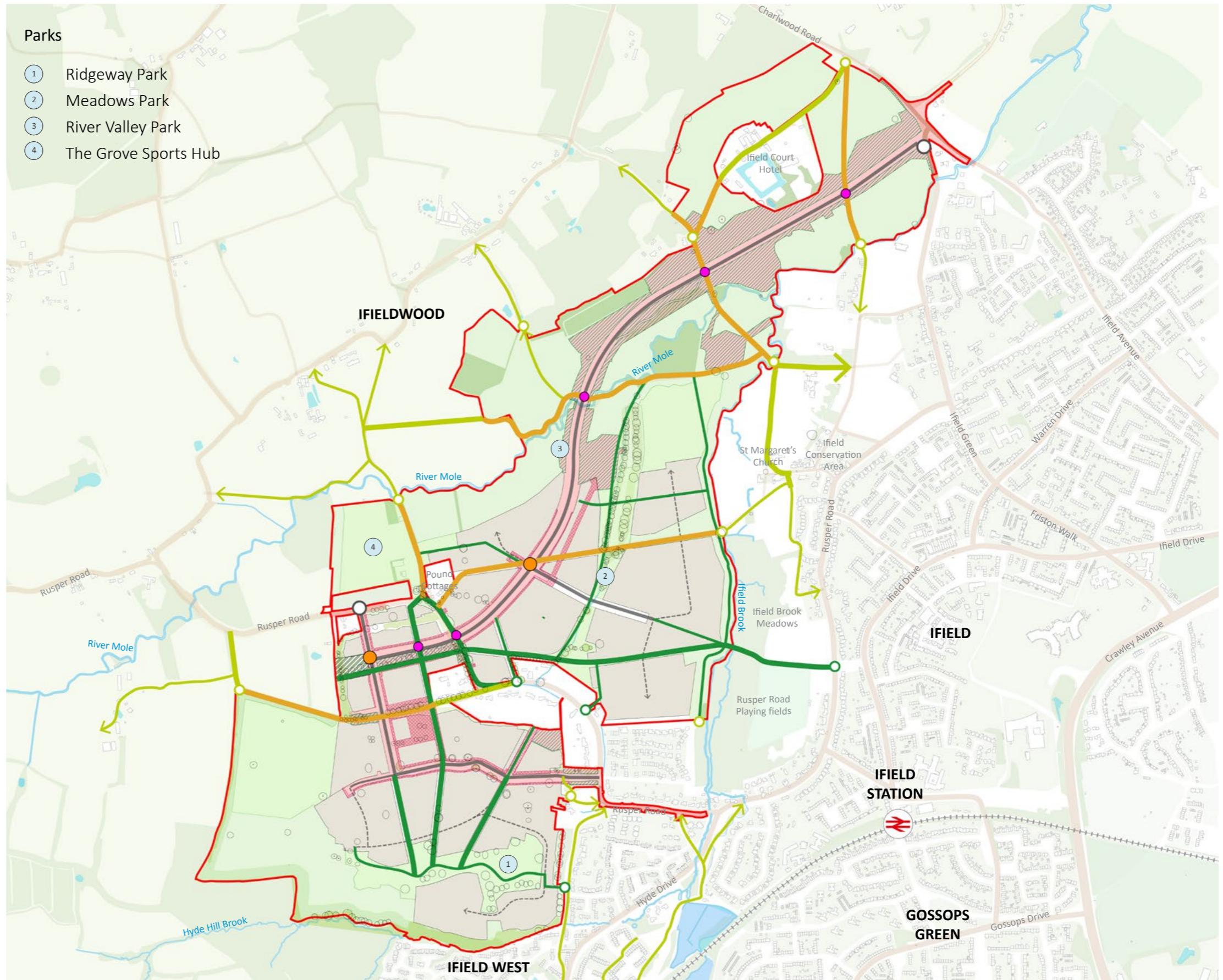


FIGURE 15 Pedestrian and Cycle Plan

3.1.2 MOBILITY HUBS

Vision and Objectives: Mobility hubs will be a key resource at West of Ifield, allowing residents to make smooth and safe transfers between different modes, enabling use of shared vehicles instead of private cars, bikes, buses, scooters or walking. The facilities provided within the mobility hub will compliment active and sustainable travel.

OPA Coding

- 1. Location of Mobility Hubs:** The three bus stops must incorporate a 'superhub' design, serving as primary mobility hub. Additional mobility hubs can strategically located to support active forms of transport.

2. Types of Mobility Hubs:

- Primary Mobility hubs:** The hub must be situated alongside a bus route, featuring an upgraded bus stop with improved seating, shelters, real-time bus updates, integrated ticketing, and facilities like secure covered bike stands, shared mobility, and car clubs.
- Secondary Mobility Hubs:** The hub must be strategically located within the community, generally providing a range of facilities which include cycle parking and car club bays. They may also include e-cycle and e-scooter hire, cycle maintenance service and EV charging.
- Mini Mobility Cluster:** Supplementary clusters of car clubs and other mobility services, such as cycle parking facilities, can be placed across the neighbourhood, providing localised access.

- 3. Car Clubs:** Every resident must have access to car club parking bays within 10 minutes walking radius of their homes, reducing the need to own a car. The proposal must provide car club bays with at least 2 spaces per mobility hub, with the potential for others elsewhere with the development.

- 4. Smart Infrastructure:** Real-time information must be available for individual users through apps and display panels at mobility hubs.

Reserved for Future Design Stages

- Designing Bus Stops:** Bus stops should be clearly visible, well-lit with good passive surveillance. They will be designed with seating and shelter to make the passenger's wait more pleasant. Additionally, it should consider the needs of people with disabilities. Opportunities should be taken to incorporate biodiversity supporting features (such as green roofs on shelters) and zero-carbon energy (such as solar panels).
- EV Charging:** Future detailed codes must demonstrate how the design can accommodate EV charging for car clubs bays, e-cycle and e-scooter.
- Vans:** Future detailed codes should demonstrate how vans could be provided as part of the car club offer, site wide.
- Flexibility:** Future detailed codes should demonstrate how the design of mobility hubs could accommodate support to other commercial services such as delivery lockers.
- Inclusivity:** Future detailed codes should demonstrate how to maximise the accessibility of mobility hubs for users with disabilities, low-income and those without smart-phones.
- Implementation:** The implementation and development of mobility hubs in a residential area involve a collaborative effort. Future detailed codes should incorporate the concept and initial plans for mobility hubs within their development proposals. The specific design, placement, and features of these hubs will be subject to approval or guidance from local authorities or infrastructure planners.



FIGURE 16 Hubs with a range of facilities such as cycle parking, e-cycle and e-scooter hire, car club bays and EV charging



FIGURE 17 Car Club Schemes to reduce private parking



FIGURE 18 Bus stops designed to be safe and secure

3.1.3 SERVICE, DELIVERY AND EMERGENCY VEHICLES

Vision and Objectives: Clear Servicing, Delivery and Emergency vehicle routes will be established throughout the street network. These will ensure requirements of utility providers, refuse collection, emergency access and logistics are met without compromising the quality of place.

OPA Coding

- 1. Access:** Service, delivery and emergency vehicles routes must be designed to provide direct access without compromising the quality of the place.
- 2. Zones:** Drop-off and pick up zones must be defined and provided in safe and secure locations.
- 3. Vehicle Manoeuvring:** Designated turning zones suitable for reversal and turning of vehicles must be provided on streets that have no through way.
- 4. Pedestrian Priority:** Service roads must be designed and regulated to prioritise pedestrian safety and comfort. This might include wider pedestrian walkways, designated crossings, traffic calming measures, clear signage, speed limits, and other visual or physical cues that emphasize pedestrian safety and right-of-way.
- 5. Local Centre:** Proposals must demonstrate a service and delivery strategy within the local centre for non-residential uses. It must demonstrate how these activities can take place safely and conveniently without causing any obstruction or congestion.
- 6. Bus gate:** The bus gate must utilise cameras instead of a bollard system. This allows emergency vehicles through without any additional technology on the vehicles, ensuring quick and unimpeded emergency access.

Reserved for Future Design Stages

- Refuse:** Future detailed codes should demonstrate approaches to manage refuse and collection, as part of the servicing and delivery strategy. These must prioritise the ease of recycling for residents or businesses.
- Logistics:** Future detailed codes should demonstrate sustainable and effective approaches to manage logistics, as part of a wider servicing and delivery strategy. This should include implications of frequency and timings of servicing any given premises. They may include freight consolidation centres and the use of alternative vehicles such as cargo-bicycles.



FIGURE 19 Logistics - Alternative vehicles for last-mile connectivity



FIGURE 20 Different approaches to manage logistics

3.1.4 STREET HIERARCHY, TYPES AND CHARACTER

Vision and Objectives: The street network will be a high quality piece of public realm that accommodates movement modes and places to stop, sit and play. The streets will be well overlooked, safe and secure.

A clear street hierarchy will ensure that the development is well-connected and legible. The different street types and character areas will be defined by consideration of speed limits, modes of travel, surrounding uses and landscaping.

OPA Coding

1. Street Hierarchy: The CWMMC and the Primary Street layout have been developed as detailed proposals. Any additional primary streets must be designed in accordance with the Movement and Access Parameter Plan. Secondary and tertiary streets be provided broadly in alignment as shown illustratively in Figure 25.

- **Primary Streets** must facilitate access to secondary or tertiary streets on both sides through the junctions. They're intended for 30mph traffic and designed to meet the standards set by the Local Highway Authority for adoption.

- **Secondary Streets** must provide access and movement through development parcels, designed for a 20mph speed limit.

- **Tertiary Streets** must connect to primary and secondary streets, incorporate traffic calming measures to enhance pedestrian and cyclist safety. For the detailed design, please refer to the subsequent pages.

2. Street Typologies: Sustainable transport modes are to be prioritised across all typologies, to encourage their use over cars. They must comply with Manual for streets or latest guidance.

3. Pedestrian and Cycle Priority: At junctions and crossings along primary and secondary streets, priority must be clearly designated for pedestrian and cycle users through distinct markings and raised table areas.

4. Traffic Calming Measures: Crossings must be a mixture of controlled and uncontrolled, with localised narrowing of the carriageway

recommended where appropriate.

5. Pedestrian and Cycle Separation: Zones must be provided to separate pedestrian and cycle routes from the carriageway for CWMMC, primary and secondary streets. They must comply with LTN 1/20 or latest guidance. They could include SUDs.

6. Boundary Treatments: In all cases edge treatments must clearly define the edge of the highway and provide a distinction between public and private realm.

7. Utilities and Infrastructure: Utilities must be laid below footway, cycleway and carriageway areas. Planning for tree planting areas clear of utility runs must be established from the project's inception and consistently safeguarded throughout all development phases. It's strictly prohibited to lay utilities along the length of planted areas. In rare cases where crossing utility runs becomes an absolute necessity, any exclusion of trees must be justified, taking into account for tree placement and root growth.

8. Inclusive Design (Accessibility for people with disabilities): Streets must be designed inclusively to meet the needs of all user groups conforming the current legislation. Paths must be of a suitable width to allow wheelchairs, prams and other users to pass. To ensure individuals have resting opportunities, seating must be strategically placed. This includes areas with steep gradients that can't be kept below 1:21, as well as at significant landmarks, bus stops, or locations with high pedestrian activity.

9. Street Trees: Refer to section 3.2.10 for coding relating to street trees and hedgerows.



Reserved for Future Design Stages

- **Materiality & Character:** Streetscape elements must be robust and hard-wearing and must comply with all relevant Local Authority streetscape design guidance where streets are to be adopted. Character of materials should vary by street type and character area, while still maintaining an overall sense of cohesiveness.
- **Lighting & Trees:** Street lighting must be coordinated with street trees from the outset and if necessary additional fittings added to ensure even coverage.
- Sensitive lighting design to follow guidance and principles provided in the BCT and Institution of Lighting Professionals (ILP) Guidance Note 08/23 'Bats and artificial lighting at night' (or as updated).
- **Manholes & Alignment:** Manholes and covers must be neatly aligned, using recessed infill covers wherever possible.



No copyright, own image

FIGURE 21 Green streets with parking and incidental play



No copyright, own image

FIGURE 22 Dedicated pedestrian & cycle routes



@ St. Andrews Street, Uxbridge

No copyright, own image

FIGURE 23 Tree planting separating parking bays

KEY

Context

- Hybrid Planning Area
- Existing Vehicular Links
- Green Infrastructure
- Water Course
- Attenuation Ponds
- Building Zone

Details defined in Detailed Proposes

- Area Applied in Detail
- Areas included within the detailed application but where RMAS will be submitted in the future.
- Landscape delivered under the detailed element
- CWMMC
- The Primary Street

Details defined in Parameter Plan (fixed components)

- Primary Street

Details indicated in Illustrative Masterplan (flexible components)

- Indicative Secondary Street
- Indicative Tertiary Street



FIGURE 24 Shared surface residential streets

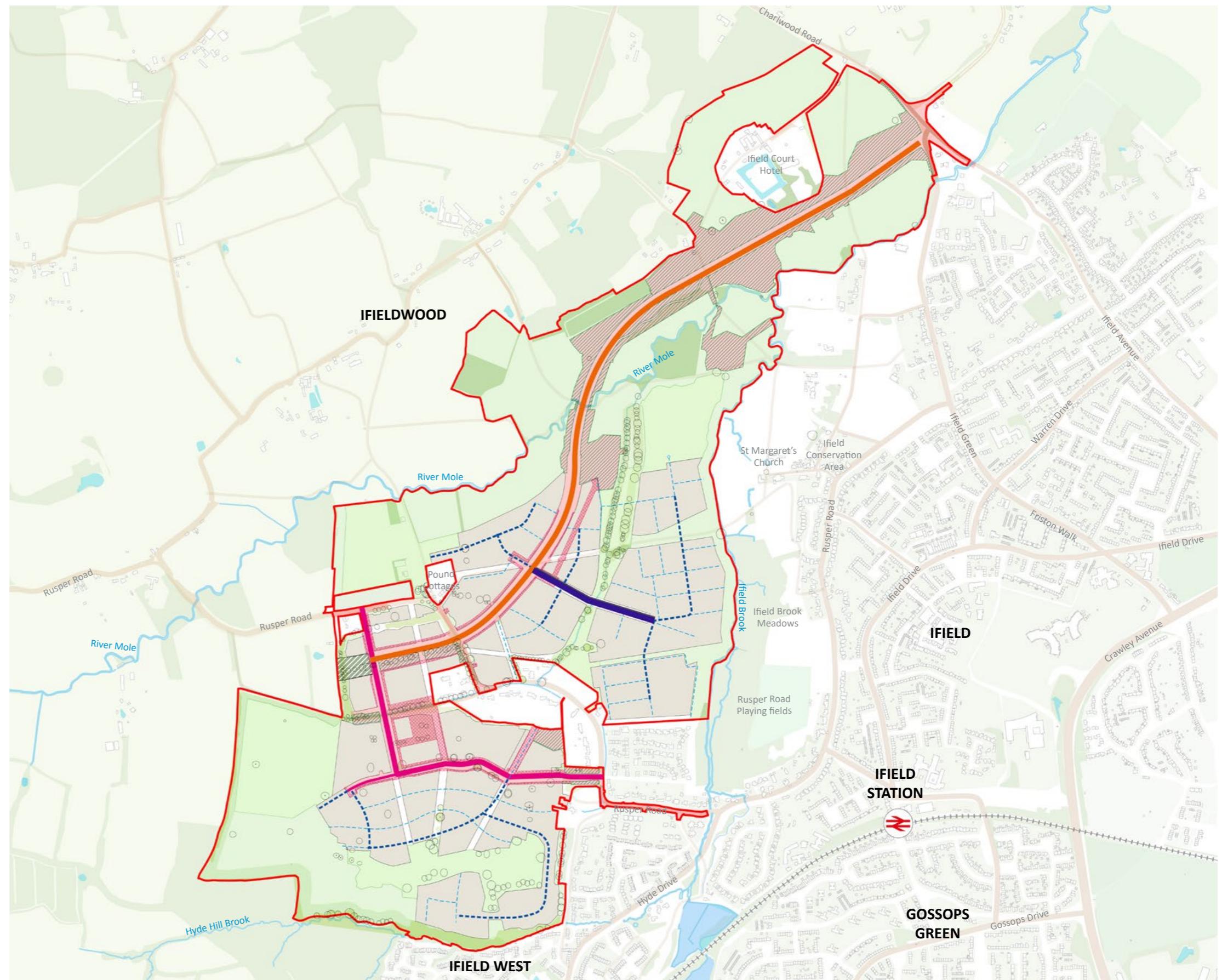


FIGURE 25 Street Hierarchy Plan

3.1.5 STREET DESIGN - SUMMARY TABLE

Type	Function	Character	Design Speed	Overall Width	Carriageway Width	Pedestrians	Cyclists	Planting	Car Parking	Kerbs
CWMMC	Proposals for the CWMMC have been developed in detail - Refer to the Design and Access Statement for further information.									
Primary Street *	Key movement routes through the development for vehicles. Connects character areas and urban centre.	Urban, green streets	20mph	31m typically Min 21.5m in Urban Centre	6.75m typically - 3.375m shared car & bus lanes Can reduce to 6m on straight sections 5m in Local Centre shared street - No bus access	2.5m footway on both sides	3m cycle path for two way cycling Cycle route on vehicle carriageway in Local Centre shared street	3-5.5m planting strips between carriageway and cycle route (50% SUDS & 50% neutral grassland) Street trees - Single stem, regular spacing	Groups of parallel parking bays located in SUDs planting strips	125(w) x 125(h) mm upstand kerbs to carriageway edge 125mm wide flush kerbs to pedestrian and cycle routes
Secondary Street	Local access within character areas, providing residential access and servicing.	Urban, green streets	15-20mph	21.5-25.5m	5.5-6m	2.5m footway on both sides	Cycle route on vehicle carriageway	2.4-5m planting strips between carriageway and footway (50% SUDS & 50% neutral grassland) Street trees - Single stem, regular spacing	Mixture of parallel and perpendicular parking bays depending on available space	125(w) x 125(h) mm upstand kerbs to carriageway edge 125mm wide flush kerbs to pedestrian and cycle routes
Tertiary Street	Pedestrian and cycle friendly shared streets with low levels of vehicle movement.	Shared spaces with pedestrian priority and traffic calming	10-15mph	7-12m	4.8m for lengths below 50m 5.5 for lengths above 50m	Mixture of single sided 2m footway and shared spaces with cycle route	Mixture of dedicated cycle routes and shared with vehicle route	Mixture of planters, verges & defensible planting at varying widths - refer to sections Street trees - Single & multi-stem, groupings and irregular spacing	Parallel parking bays depending on available space	125mm wide kerbs Small (>25mm) upstands to separate pedestrians and vehicle routes

* Local Centre Shared Street is a location specific type of Primary Street, therefore not included in this page of general design principles; refer to section 3.1.10 for further details.

3.1.6 STREET DESIGN - PRIMARY STREETS

Primary Streets within the development will be the main connecting roads for vehicle users, as well as providing separate walking and cycling routes to help prioritise sustainable transport modes.

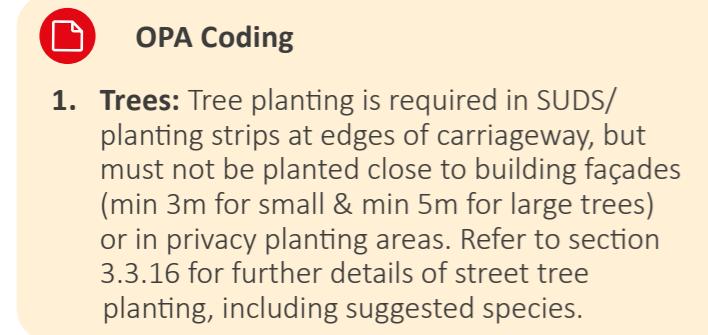


FIGURE 26 Street trees & SUDs planting

Type	Function	Character	Design Speed	Overall Width	Carriageway Width	Pedestrians	Cyclists	Planting	Car Parking	Kerbs
Primary Street	Key movement routes through the development for vehicles. Connects character areas and urban centre.	Urban, green streets	20mph	31m typically	6.75m typically - 3.375m shared car & bus lanes Can reduce to 6m on straight sections 5m in Local Centre Shared Street - No bus access	2.5m footway on both sides	3m cycle path for two way cycling Cycle route on shared carriageway in Local Centre Shared Street	3-5.5m planting strips between carriageway and cycle route (50% SUDS & 50% neutral grassland) Street trees - Single stem, regular spacing	Groups of parallel parking bays located in SUDs planting strip zone	125(w) x 125(h) mm upstand kerbs to carriageway edge 125mm wide flush kerbs to pedestrian and cycle routes

3.1.7 STREET DESIGN - LOCAL CENTRE SHARED STREET

The size and character of the streetscape will change within the Local Centre, creating a more pedestrian friendly, shared space road. Vehicle access will be limited and pedestrian/cycle given priority.

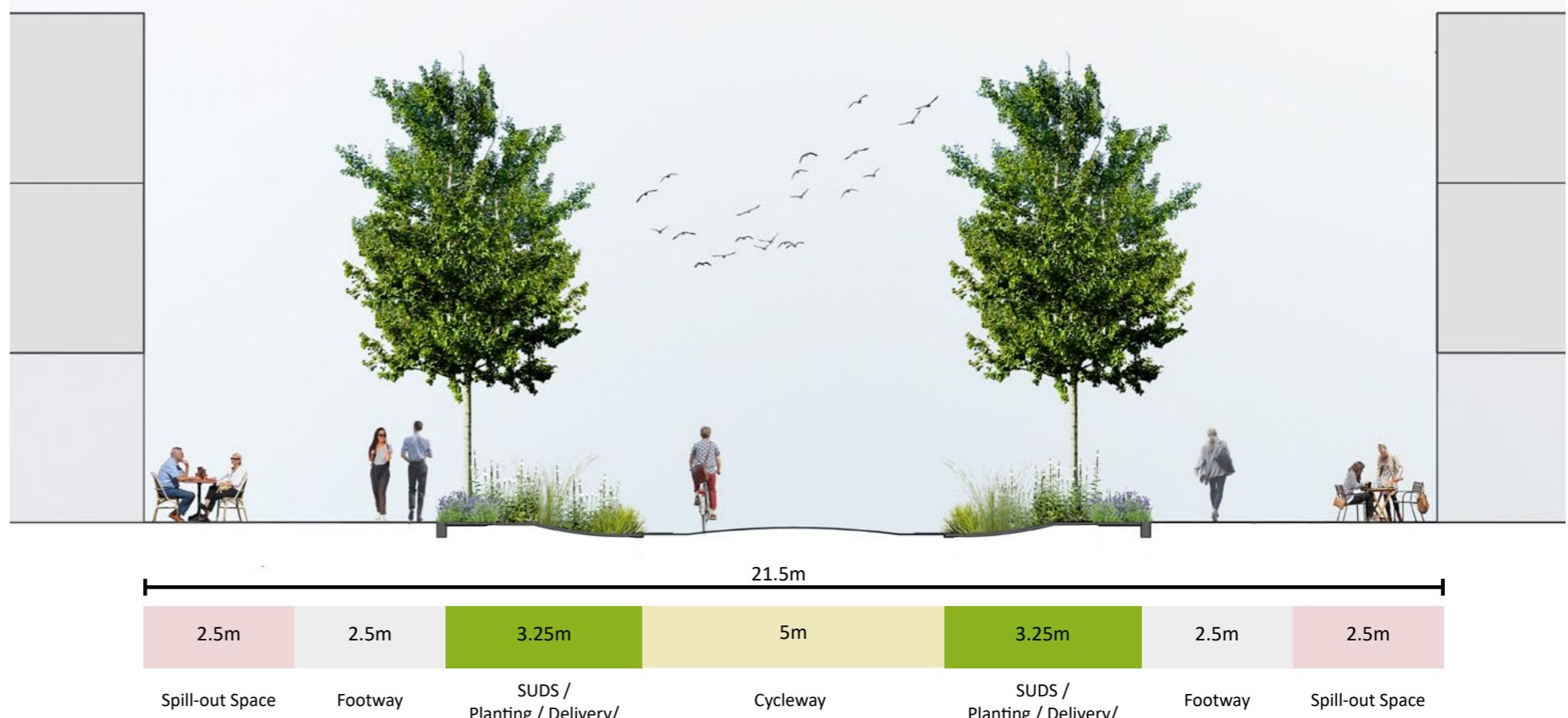


OPA Coding

- Trees:** Tree planting is required in SUDS/ planting strips at edges of carriageway.



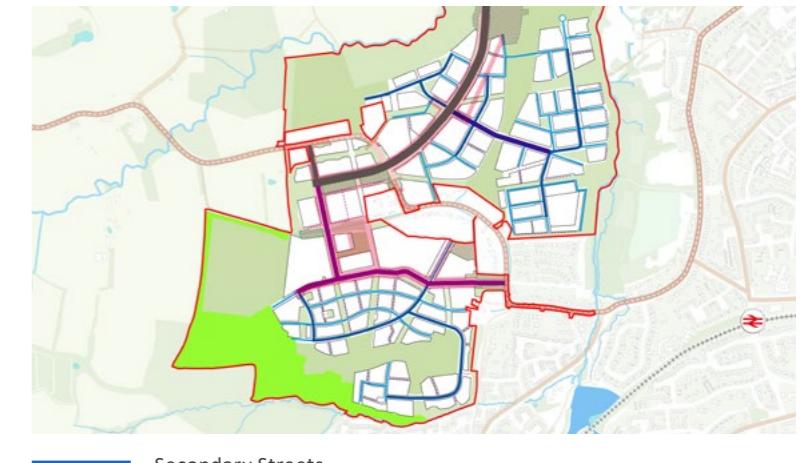
FIGURE 27 Pedestrian & cyclist priority spaces



Type	Function	Character	Design Speed	Overall Width	Carriageway Width	Pedestrians	Cyclists	Planting	Car Parking	Kerbs
Local Centre Shared Street	Shared street running parallel to the primary vehicle route through the Neighbourhood Centre	Urban, green streets, Pedestrian & cycle priority	20mph	Min 21.5m	5m - Made up of 4m carriageway with 500mm buffer strips at edges to narrow perceived width and slow vehicles - No bus access	2.5m footway on both sides	Cycle route on shared carriageway Cycle parking located in SUDs planting strip zone	3-3.5m planting strips between carriageway and cycle route (50% SUDs & 50% neutral grassland) Street trees - Single stem, regular spacing	No parking. Delivery /drop-off located in SUDs planting strip zone	500mm wide buffer strip to carriageway edges (flush tactile paving) 125mm wide flush kerbs to pedestrian routes

3.1.8 STREET DESIGN - SECONDARY STREETS

Secondary Streets will connect residential plots to Primary Roads and the CWMMC. These streets will be very green in character, with reduced vehicle speed limits and on road cycling.



Note: Section is illustrative of design principles; width and layout will vary by location

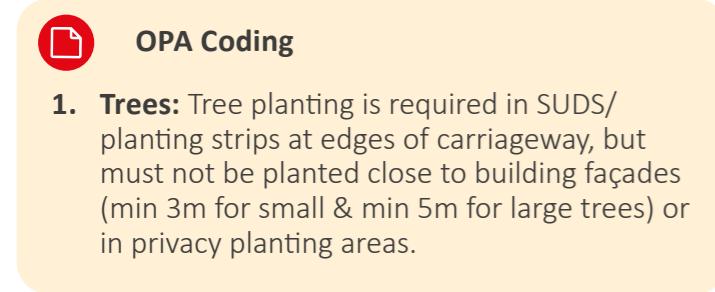


FIGURE 28 Secondary street arrangement

Type	Function	Character	Design Speed	Overall Width	Carriageway Width	Pedestrians	Cyclists	Planting	Car Parking	Kerbs
Secondary Street	Local access within character areas, providing residential access and servicing.	Urban, green streets	15-20mph	21.5-25.5m	5.5-6m	2.5m footway on both sides	Cycle route on shared carriageway	2.4-5m planting strips between carriageway and footway (50% SUDS & 50% neutral grassland) Street trees - Single stem, regular spacing	Mixture of parallel and perpendicular parking bays depending on available space	125(w) x 125(h) mm upstand kerbs to carriageway edge 125mm wide flush kerbs to pedestrian routes

3.1.9 STREET DESIGN - TERTIARY STREETS

Tertiary streets will give increased pedestrian priority, vehicle access will be less frequent and at lower speeds.

Spaces will be shared, with traffic calming measures and more subtle delineation of areas.

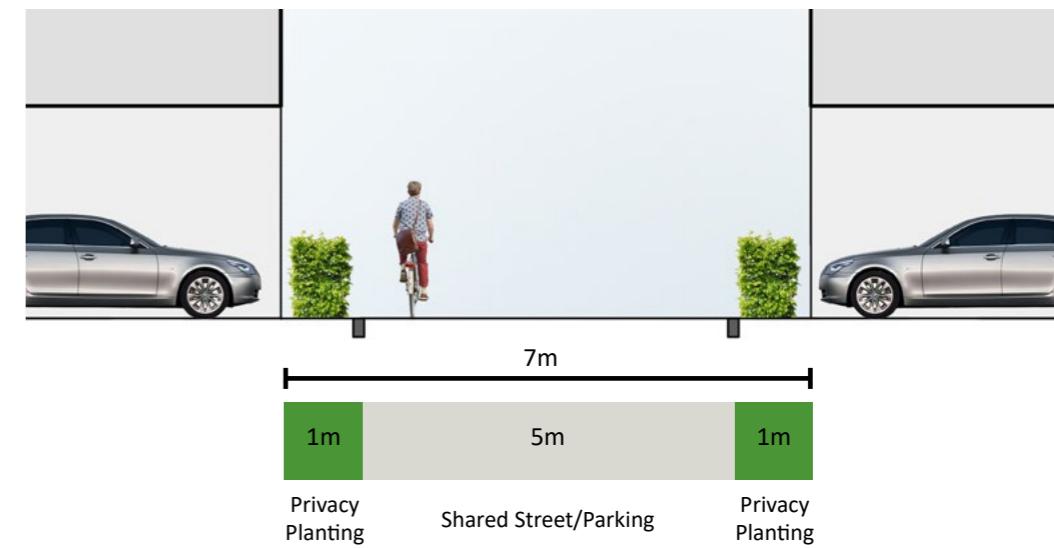
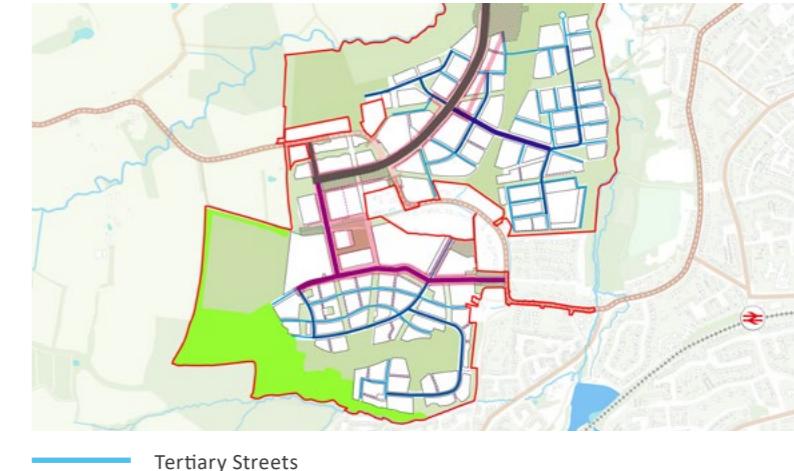


OPA Coding

- Trees:** Tree planting is required in SUDS/planting strips at edges of carriageway, but must not be planted close to building façades (min 3m for small & min 5m for large trees) or in privacy planting areas.



FIGURE 29 Shared street with planted verges



Note: Section shows garages with electric charge ports in the ground floor of properties. 7m shared street is proposed to enable use of the garages and prevent on street parking. Privacy planting would not be proposed in front of garage entrance sections- shown to illustrate the full street design.

Type	Function	Character	Design Speed	Overall Width	Carriageway Width	Pedestrians	Cyclists	Planting	Car Parking	Kerbs
Tertiary Street	Pedestrian and cycle friendly shared streets with low levels of vehicle movement.	Shared spaces with pedestrian priority and traffic calming	10-15mph	7-12m	4.8m for lengths below 50m 5.5 for lengths above 50m	Mixture of single sided 2m footway and shared spaces with vehicle and cycle route	Mixture of dedicated cycle routes and shared with vehicle route	Mixture of planters, verges & defensible planting at varying widths - refer to sections Street trees - Single & multi-stem, groupings and irregular spacing	Parallel parking bays depending on available space	125mm wide kerbs Small (>25mm) upstands to separate pedestrians and vehicle routes

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3.1.10 CAR PARKING

Vision and Objectives: The Car parking strategy for West of Ifield aims to minimise the impact of parked cars on the public realm and streetscape.

Parking provision will be determined by the level of accessibility to active and public transport.



OPA Coding

1. Requirements: Proposals must use parking levels identified within the Transport Assessment and County Council standards.

2. Provision: It must be demonstrated how car parking levels have been provided in line with Transport Assessment, with specific provisions responding to levels of connectivity by non-car modes. This is expected to include approaches to off-plot parking and shared parking, in order to encourage the use of alternative modes and making efficient use of land.

3. Flexibility: It must be demonstrated how the future reuse of car parking areas (shared and on street) has been considered. The Transport Assessment proposes the use of 'interim' parking areas that provide parking for short-medium term demand but could be re-purposed as public realm or development land as parking demand reduces in the longer term.

4. Street scene: Car parking must be clearly demarcated without causing visual intrusion or negatively impacting on place-making and the street scene.

5. Electric vehicle (EV) Charging: Public and private electric car charging points must be integrated within the design of streets and buildings. Public EV charging stations should be strategically placed in key public spaces with on-street parking for convenient accessibility. Private EV charging can be discreetly located within garage, or where no garage is provided, on a side elevation,

or integrated within the entrance porch for houses.

6. Non-residential Car Parking: Proposals must align with Transport Assessment and employ flexible shared parking solutions in mixed-use and employment areas.

7. Residential Parking: Private parking options for residential uses are illustrated in Figure 30 to Figure 37 and indicate a range of acceptable options.

8. On Street Parking: The use of bollards to manage informal on street parking must be limited but where necessary timber bollards should be used. Bollards should generally be limited to areas where other landscape features would not be appropriate, for example junctions and crossing points.



Reserved for Future Design Stages

- Parking Management:** Future detailed codes should develop a parking management plan which aims to ensure parking bays are utilised by the intended user, avoid informal parking, and protects the public realm.

Apartment/Maisonette - Parking can either be decked over and gardens provided (preferred) or within a well landscaped courtyard

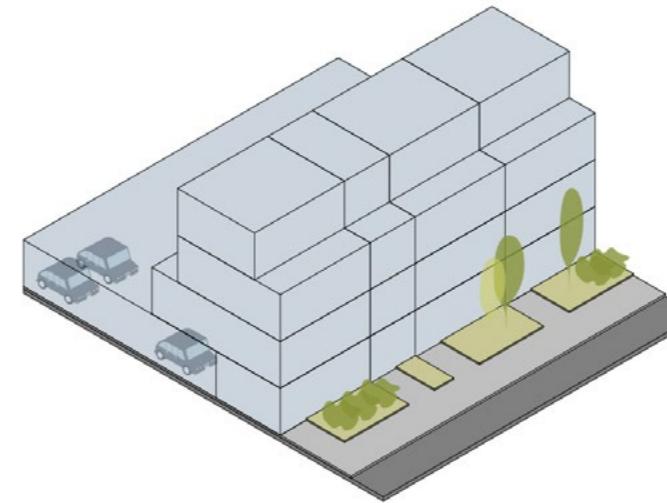


FIGURE 30 Decked-over parking



9. Decked-over Parking: This arrangement is primarily used for apartments in higher-density areas. Direct views into the car park must be minimised. Landscaping around and on top of the decked structure must be incorporated to maintain visual appeal.

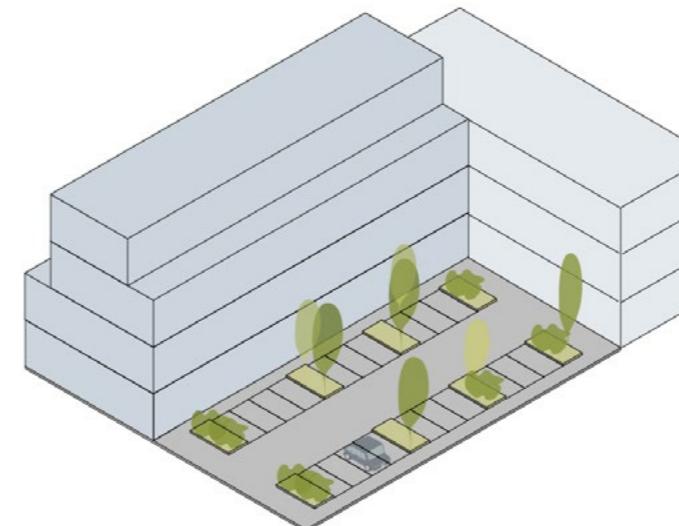


FIGURE 31 Parking court



10. Parking court: It must be designated marked bays, which are overlooked by apartments and apartment entrances and partly enclosed by buildings. Tree planting must be incorporated to enhance visual appeal.

On plot allocated

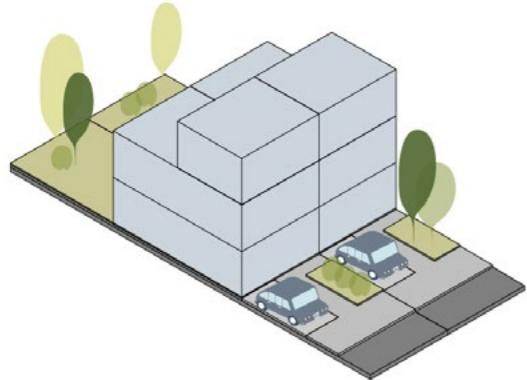


FIGURE 32 Front parking

- 11. Front parking:** Parking bays must be at least 5 metres long. Front gardens must be incorporated to avoid vehicles dominating the street view.

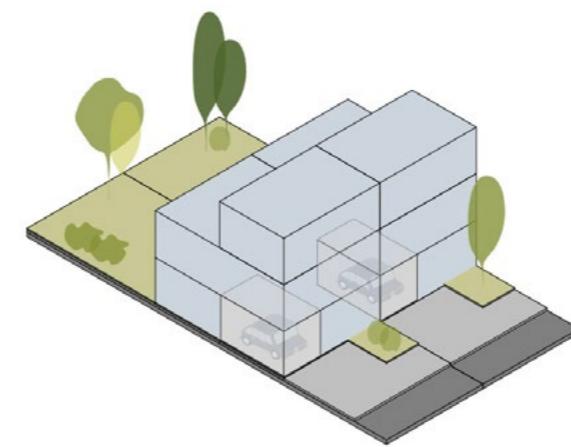


FIGURE 34 Built into front of the house

- 13. Built into front of the house:** driveways must be at least 5 metres long or less than 2 metres to preclude parking. Internal garages or car ports must be at least 3.5 metres x 6 metres.

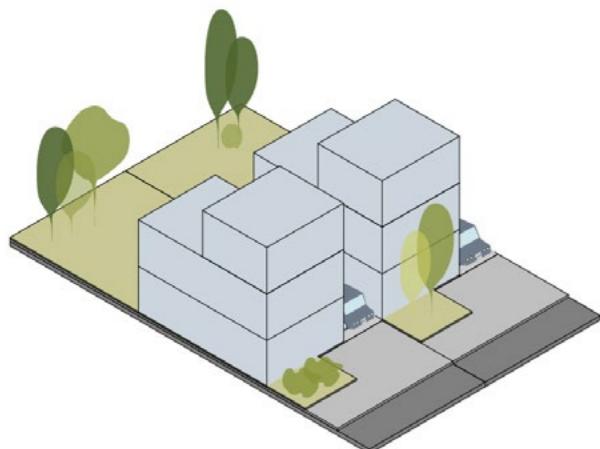


FIGURE 33 Between houses

- 12. Between houses:** Building line must be set close to the back edge of pavement to discourage parking in front of the building line. The arrangement must also accommodate viable tree planting. Additionally, an extra bay or garage maybe located behind drive through.

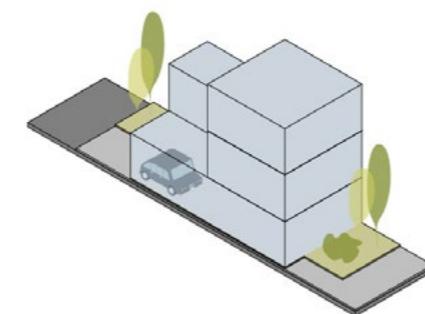


FIGURE 35 Built into back of the house

- 14. Built into back of the house:** Ideally, there must not be more than 6 units in a row without a break. The access road to the parking must be from the rear of the property and at least six meters wide. Spaces above the garages must include accommodation or amenities to ensure natural surveillance of the street

Off plot allocated

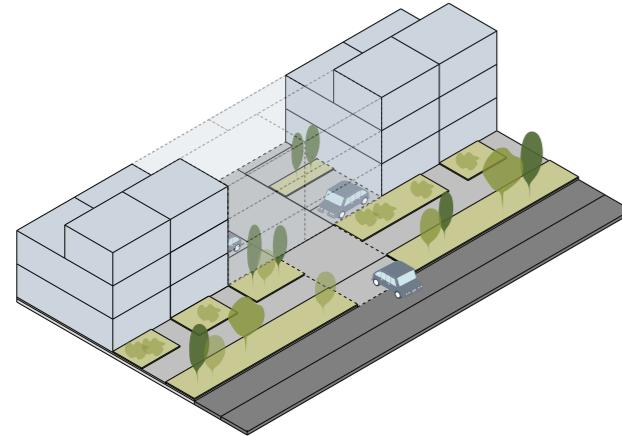


FIGURE 36 Mews Court

15. Mews Court: It must be overlooked and must not contain more than 18 spaces (including any parking arrangements), with generous planting and trees provided to enhance the quality of the space. The parking bays should feature block surfacing using the same block, contrasting block, or tarmac. Thermoplastic

(white lining) marking must not be used.

Off plot non-allocated

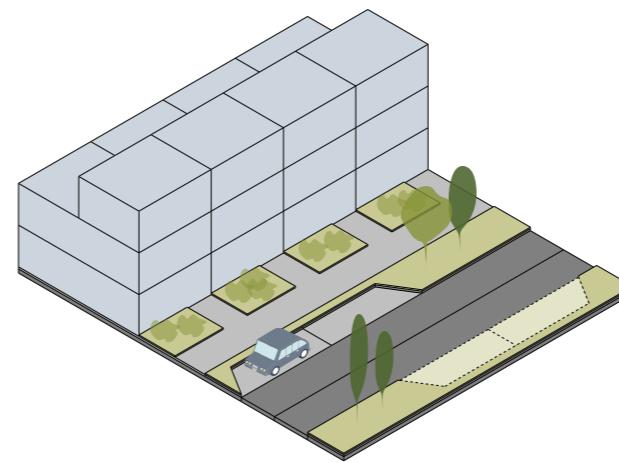


FIGURE 37 On-street car parking incorporated tree planting

16. On Street Parking: It must incorporate tree planting to enhance the streetscape. Ideally, there should be no more than four parallel spaces without a tree break or two tandem street spaces without a tree. Parking spaces must be incorporated within designated areas on the carriageway.

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3.1.11 CYCLE PARKING

Vision and Objectives: At West of Ifield secure cycle parking will be incorporated into every property and shared storage designed into residential streets and non-residential areas.

Ample public and visitor parking will be designed into all public spaces and play areas and co-located with public transport hubs.



OPA Coding

- 1. Cycle Parking Requirements:** Proposals must use the cycle parking levels identified within the Transport Assessment. Cycle parking must be provided at all homes, with shared visitor parking provided at the local centre, employment area, mobility hubs and throughout residential areas.
- 2. Layouts:** Cycle parking must be provided in prominent locations, which are easily accessible from the cycle route network. Stands and shelters must be located in such a way so as not to obstruct pedestrian traffic.
- 3. Safety:** Safe cycle parking and storage must be provided near key public spaces, public facilities and transport nodes.
- 4. Shared Storage:** Shared cycle storage must be provided across commercial, business and residential uses. It must be located close to the entrance/exit of buildings and safe and overlooked. Uncovered storage should only be used for short term visitor parking.
- 5. Personal Secure Storage:** Personal secure cycle storage must be provided for single dwelling houses. The storage must be easily accessible on a daily basis to encourage cycling and must not be located in rear sheds.
- 6. Cycle Parking Types:** Cycle parking provision must accommodate various types of bicycles and cater to the needs of all cyclists.



Reserved for Future Design Stages

- Cycle Storage Design:** Future detailed codes should demonstrate how cycle storage has been designed to provide ample lighting and as a prominent facility using high quality materials, in order to encourage it to be seen as an attractive place to leave cycles.
- Cycle Amenity:** Future detailed codes should consider how the provision of wash-down facilities have been incorporated within communal storage appropriate to building use, and the needs of the people using it.

Public/Visitor Cycle Parking

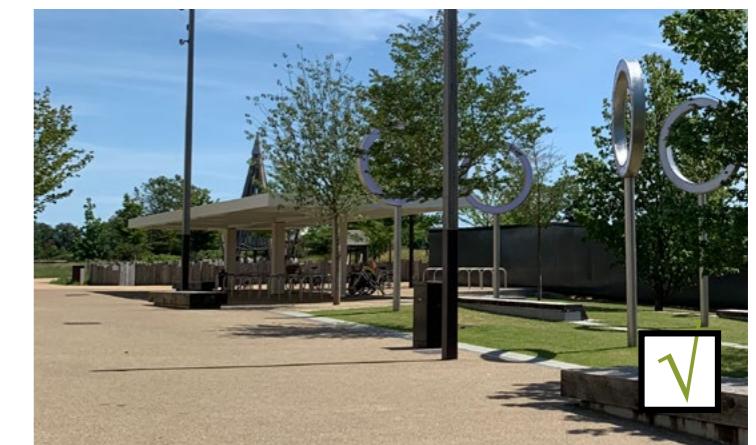


FIGURE 38 Shared bike storage along cycle route



FIGURE 39 Sheffield stand able to accommodate a cargo bike



FIGURE 40 Cycle Parking located close to shop fronts

Secure Storage for Individual House

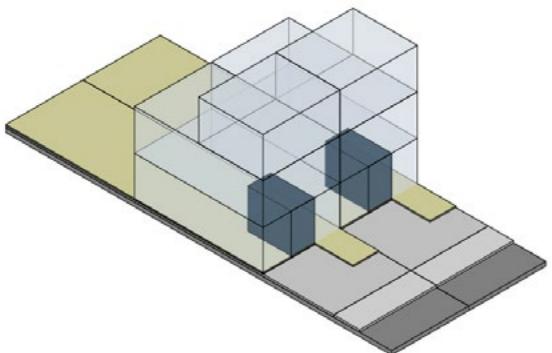


FIGURE 41 Built into the House - Garage

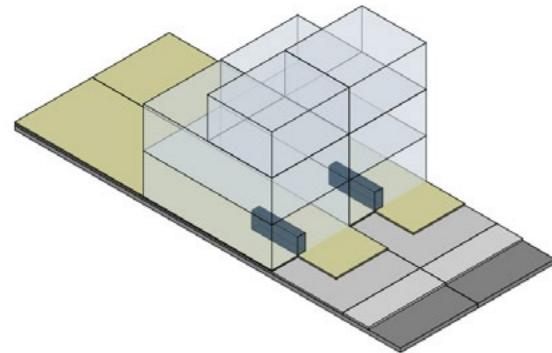


FIGURE 42 Built into the House - Hallway/ Entrance Porch



FIGURE 44 Bike Storage within private garages



FIGURE 45 An individually sheltered bicycle storage area situated at the forefront of the residence



FIGURE 46 Cycle storage in a garden shed occupies a large part of garden space and is not secure or easily accessible

Secure Storage for Apartment Building

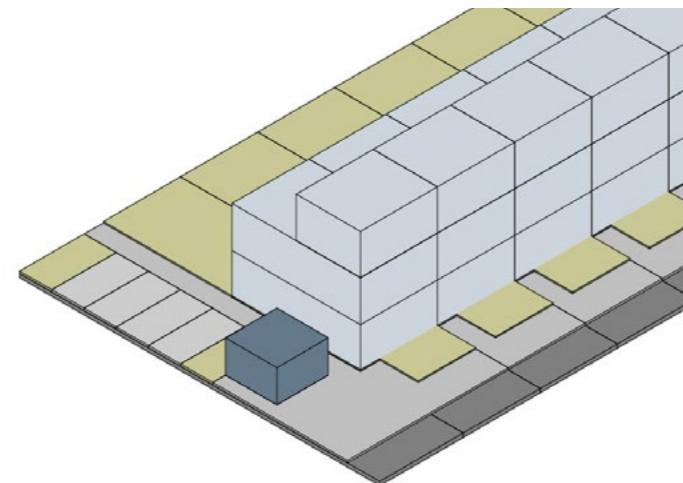


FIGURE 43 End of Street - Collective Box



FIGURE 48 End of Street - Collective Box



FIGURE 47 Cycle storage is incorporated into the curtilage of buildings.

3.2 LAND USE

Vision and Objectives: West of Ifield will be a vibrant mixed use neighbourhood, with land uses distributed to connect to and compliment existing areas and help integrate the place, create variety and activity. The proposal must provide the mix of uses needed to create self-sustaining and adaptable community.

OPA Coding

- Distribution:** Land uses must be distributed to complement existing and proposed residential and landscape spaces. This involves strategically organising various types of land uses, such as commercial, employment, and residential, in a manner that harmonises with the surrounding environment, supports the functionality of the area, and contributes to its overall attractiveness and liveability.
- Topography:** The plan must consider the topography when locating uses, for example school playing fields requiring mostly flat land.
- Flexibility:** The allocation of uses on development plots must provide flexibility, to accommodate future changes.
- Active Travel:** The proposal must consider active travel catchments for local facilities. Day-to-day services, such as grocery stores, schools, and community facilities, must be within a 15 minutes active travel distance of the people that serve.
- Co-locating Uses:** The proposal must co-locate uses which are used at different times of day can promote public realm viability throughout the day and evening, improving natural surveillance in a place.
- Local and National Policy:** Land use planning must consider uses required by local and national policy, related to proposed homes and site areas.

Reserved for Future Design Stages

- Detailed Land Use plans must be drawn up for each Character Area.

LAND USE AREAS

Land Use	Total Area (ha)
Residential	44
Employment	6.15
Mixed Use	5.26
3FE Primary School	2.4
6FE-8FE Secondary School	9.29



FIGURE 49 Integrating land uses to create variety and activate the public realm [1]



FIGURE 50 Integrating Active Travel [4]



FIGURE 51 Co-locating Uses [5]

KEY

Context

- Hybrid Application Area
- Existing Vehicular Links
- Green Infrastructure
- Water Course
- Attenuation Ponds
- Building Zone

Details defined in Detailed Proposes

- Area Applied in Detail
- Areas included within the detailed application but where RMAS will be submitted in the future.
- Landscape delivered under the detailed element

Details defined in Parameter Plan (fixed components)

- Residential
- Employment
- Flexible Employment/Residential
- Mixed-use
- Secondary School
- Primary School
- Residential also considered appropriate for Gypsy and Traveller pitches

Details indicated in Illustrative Masterplan (flexible components)

- Indicative Plot Layout

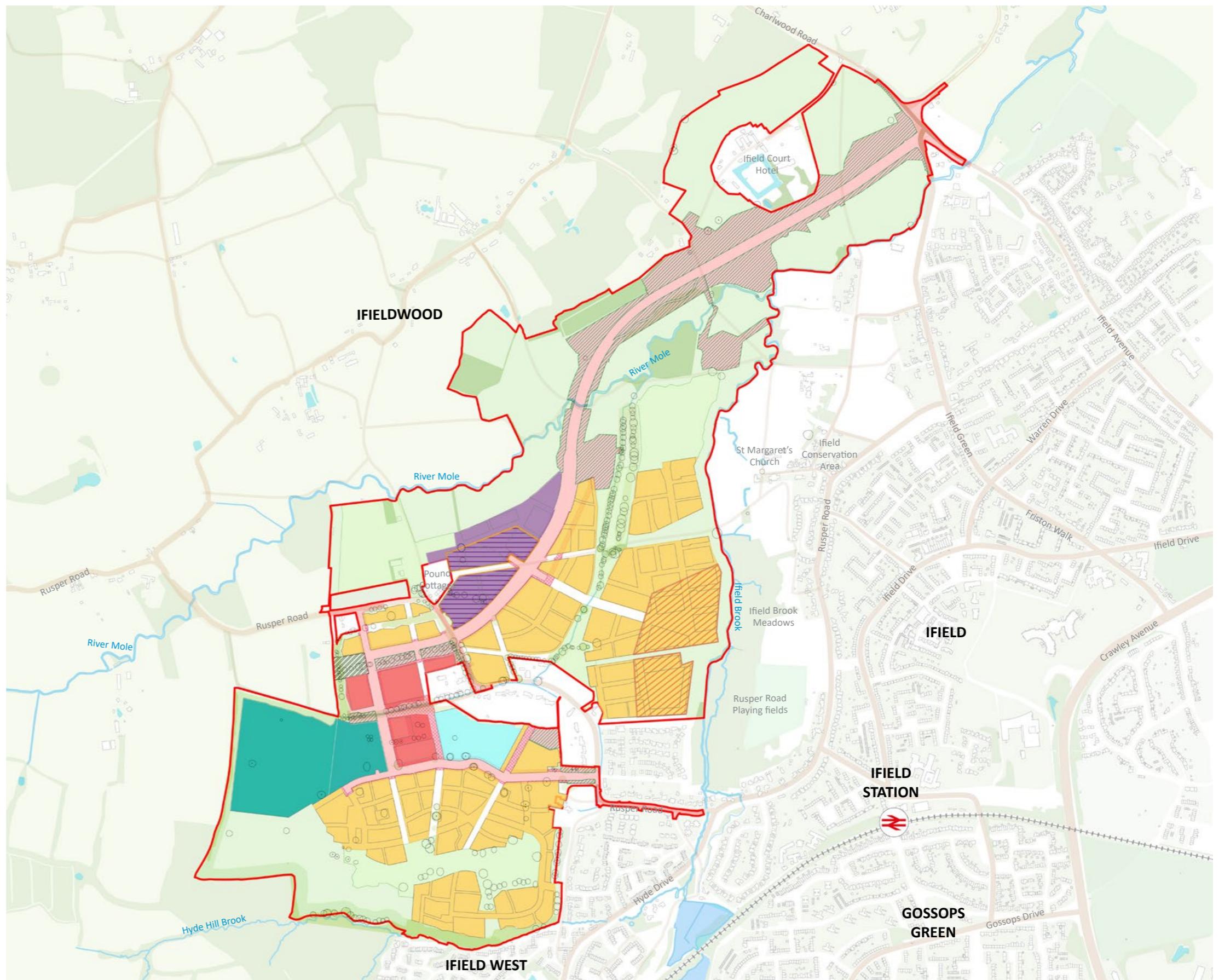


FIGURE 52 Land Use Plan

3.2.1 RESIDENTIAL

Vision and Objectives: West of Ifield will be made up of walkable residential neighbourhoods, with every resident having access to parks, open space, active travel and local facilities.

OPA Coding

- Residential Requirements:** All residential areas and class use (C2, C3 and Sui Generis) identified on the Land Use Parameter Plan must be met. They must be provided within the relevant phase to ensure provision within West of Ifield.
- Active Travel:** Residential uses must be located within 10 minutes of walking distance from active travel provision. Every resident must have easy access to parks and open space amenities.
- Proximity to Local Facilities:** Residential uses must primarily be located within 15-minute of walking distance from local centre as shown in Figure 54. Additionally, mini mobility clusters can be established near the edge of the 15-minute walking zone to enhance convenient travel and accessibility even further.
- Tenure:** Overall developments must include a mix of tenures and house-types, including market and affordable homes for sale and rent, and specialist housing such as accommodation for older care. Each individual proposal must contribute to the overall mix, ensuring a balanced and comprehensive community.

Reserved for Future Design Stages

- Specialist Housing:** Specialist housing (eg extra care) needs to be located within walking distance of the local centre and should have easy vehicular and public transport access.
- Self-build / Custom-build Homes:** There is potential to cater for self-build/custom-build homes, which should be designed to the same standards as other residential. They should be located as individual plots or small clusters.
- Gypsy and Traveller Sites:** The Gypsy and Traveller Site with up to 15 pitches will be located within the residential land use category. The site must be easily accessible via primary roads, accessibility to local facilities, services and transport links, relatively flat and within relevant acoustic constraints (see section 3.2.5 for further details).
- Noise Contour:** Residential dwellings and private gardens would not be placed within the Site areas that sit within the 60 dB(A) Leq,16hour contour that is applicable at the time of the reserved matters planning application.

KEY

Details defined in Parameter Plan (fixed components)

- Residential
- Flexible Employment and Residential zone
- Residential plots located within 60dB noise contour zone
- 60dB noise contour (as 2025)

